



June 10, 2022

Mr. Joshua Berry, AICP  
Senior Planner  
Cranston City Hall  
869 Park Avenue  
Cranston, RI 02910

Re: Development of the former Trolley Barn Site Cranston, RI  
Traffic Study Peer Review  
Fuss & O'Neill Reference No. 20200078.T20

Dear Mr. Berry:

Fuss & O'Neill has conducted a review of the traffic study and associated Master Plan relating to the proposed development at 777 Cranston Street in Cranston, RI, also referred to as the former Trolley Barn site. The development consists of an AutoZone and warehouse, a car wash, a fast-food restaurant with a drive-thru window, and a convenience market/gas station.

Materials Reviewed:

1. Traffic Impact Study completed by BETA Group, Inc. titled "Trolley Barn Plaza" dated July 2021, Revised April 2022.
2. Master Plan Set entitled "Preliminary Plan Submission; Trolley Barn Plaza; 777 Cranston Street, Cranston, Rhode Island" prepared by DiPrete Engineering dated 5/24/2022 (15 sheets)

We offer the following comments:

Traffic Impacts

1. We concur that upon implementation of appropriate off-site mitigation, the proposed development will have no significant impact on traffic operations within the study area. We believe the analysis provided in the traffic study to be conservative, as the trip generation does not assume a reduction for pass-by trips or internal capture.

We recommend that the applicant conduct traffic counts at the site driveway and other intersections where signal re-timing is proposed during the opening month of the development. This will ensure that any off-site mitigation is required will be based on actual trip generation instead of estimates.

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## On-Site Circulation and Parking

1. The proposed development is designed exclusively for vehicle traffic. Given the site's proximity to transit (RIPTA Route 30 Bus) and the Washington Secondary Bike Trail, as well as its location adjacent to densely populated residential neighborhoods, this site should be designed to serve all roadway users. We recommend a reduction in vehicle parking, the construction of sidewalks on site, and the inclusion of bicycle parking onsite.
2. Section 17.28.010(A)(5) – Pedestrian access from the right-of-way and on-site is not currently depicted. Although most customers are anticipated to be in passenger vehicles, it is important that there are accessible pedestrian routes around the site, and also clear access to dumpster for employees.
  - a. Recommended pedestrian access routes are depicted in the sketch Site Circulation Markup, enclosed.
3. Section 17.28.010(B)(4) – The Fast Food driveway opening (measuring curb to curb) is less than 60 feet from Cranston Street.
  - a. F&O understands that the City will be confirming the reference point for this measurement.
  - b. F&O recommends removing the second curb cut for the Fast Food use closest to Cranston Street. Master Plan only included one entrance for the Fast Food use.
4. Section 17.28.010(B)(4) and 17.48.010(D)(7) – There are multiple on-site driveway openings that are located within 40 feet of another driveway opening (measuring curb to curb). These proposed alignments should be revised to create clear 4-way intersections where possible.
  - a. It should be noted that the definition of “Driveway” is a private way for vehicles to move between a street and a location within a lot. Therefore, these Sections are applicable to all on-site driveway openings.
5. Section 17.28.010(B)(5) – There are multiple locations where the proposed driveway width is greater than 35 feet. These locations should be revised, or a waiver shall be requested.
  - a. Recommended revisions are depicted in the sketch Site Circulation Markup, enclosed.



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6. Section 17.28.010(B)(9) – Dumpster for Building Nos. 2 & 3 is located on the main traffic circulation drive aisle on site. This dumpster should be relocated so that trash pickup wont block traffic flow for the other buildings.
  - a. A recommend location to the northwest is proposed and depicted in the sketch Site Circulation Markup, enclosed.
7. Section 17.28.010(B)(12) and 17.48.060(D) and 17.84.150(A)(2) – The majority of the recommended layout revisions depicted in the enclosed Site Circulation Markup sketch are to improve the on-site vehicle and pedestrian safety.

We appreciate the opportunity to provide the City of Cranston with this peer review. Please do not hesitate to contact us with any questions.

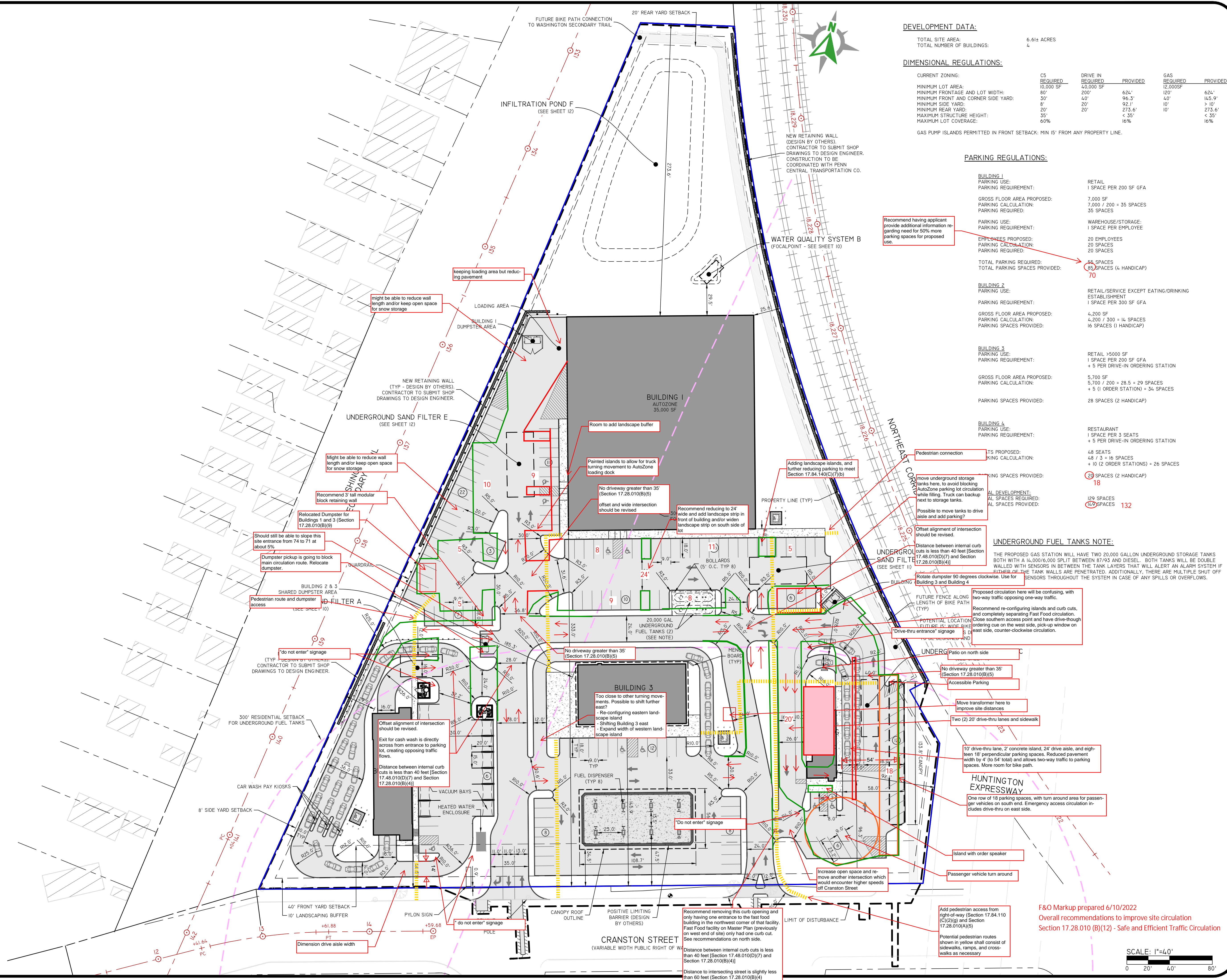
Sincerely,

Andrew G. Glines, PE  
Senior Civil Engineer

  
Katherine O'Shea, EIT  
Transportation Engineer

Enclosures: Site Circulation Markup, 2 Sheets, dated June 10, 2022





**DiPrete Engineering**

Two Stafford Court Cranston, RI 02920  
tel 401-943-1000 fax 401-464-6006 www.diprete-eng.com

**DANA R. NISSET**

REGISTERED PROFESSIONAL ENGINEER CIVIL

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| NO. | DATE       | DESCRIPTION                        | BY: | DESIGN BY: |
|-----|------------|------------------------------------|-----|------------|
| 1   | 06/17/2022 | DEVELOPMENT PLAN REVIEW SUBMISSION | NDK | NDK        |
| 2   | 06/17/2022 | PRELIMINARY PLAN SUBMISSION        | NDK | NDK        |
| 3   | 06/17/2022 | FINAL PLAN SUBMISSION              | NDK | NDK        |

DRAWN BY: NDK/MS

**SITE LAYOUT PLAN**

**TROLLEY BARN PLAZA**

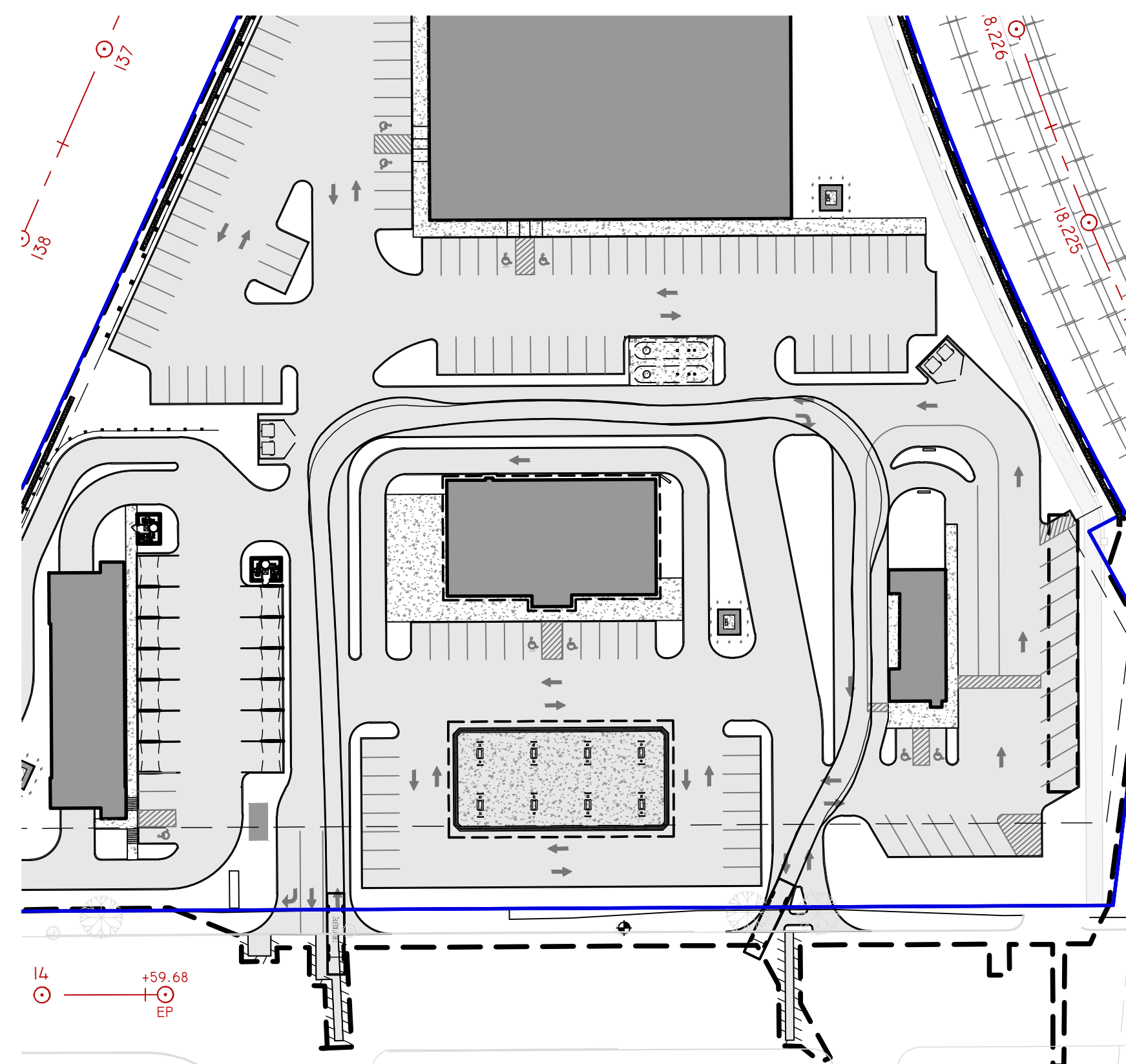
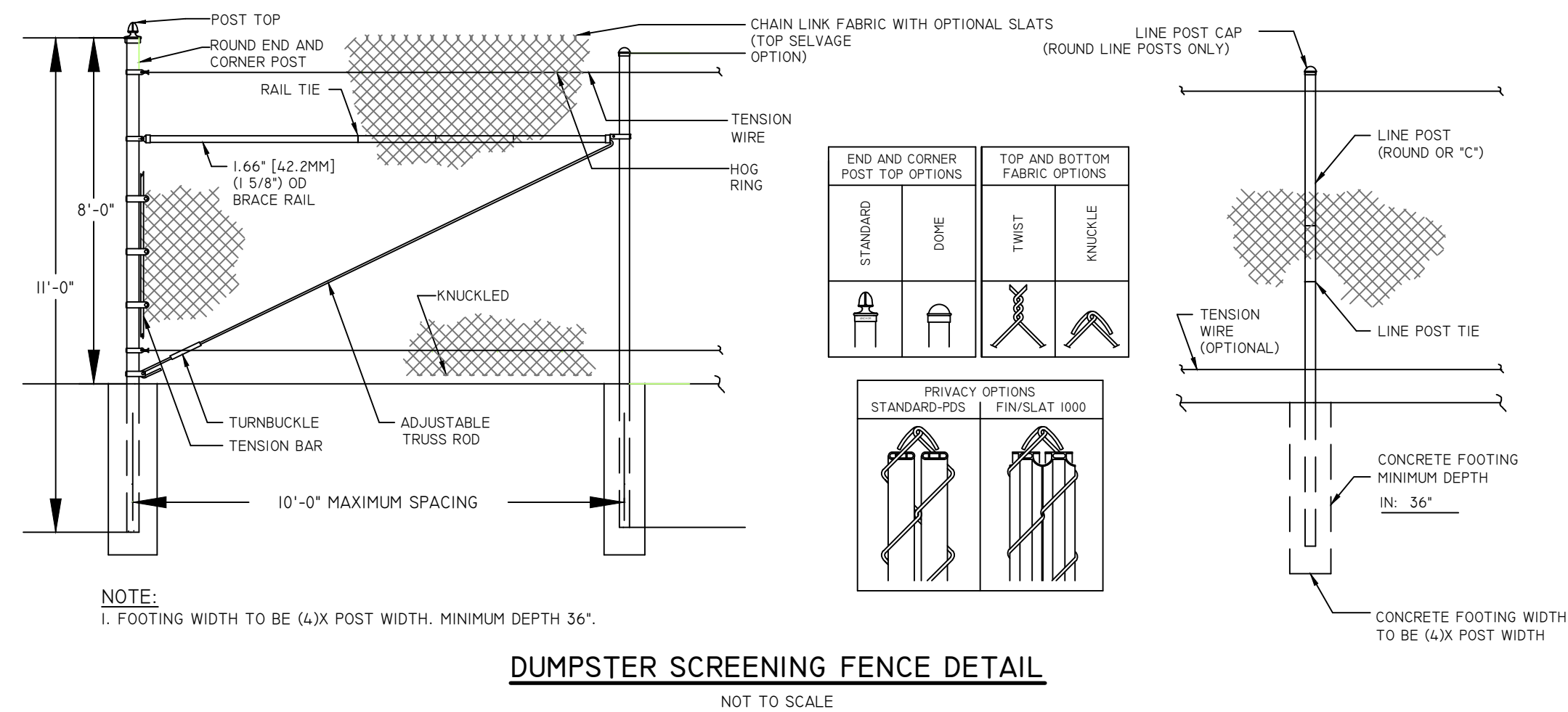
ASSESSOR'S PLAT 7 LOT 1  
CRANSTON, RHODE ISLAND

PREPARED FOR:  
**TROLLEY BARN ASSOCIATES LLC**  
C/O FIRST HARTFORD REALTY CORP.  
P.O. BOX 1270, MANCHESTER, CT 06045

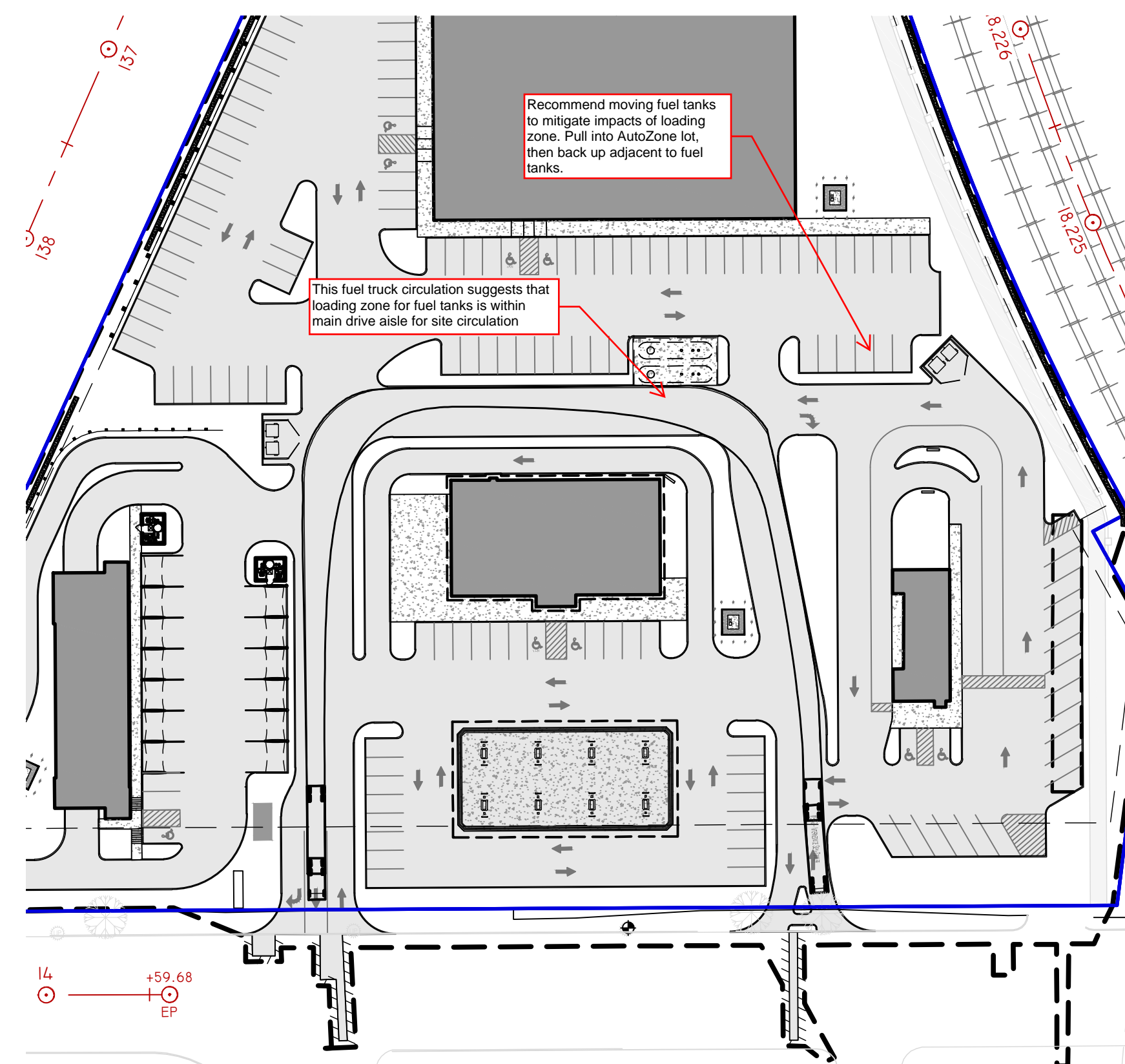
DE 2021-001-2021-001 COPYRIGHT 2022 BY DIPRETE ENGINEERING ASSOCIATES, INC.

SHEET **6** OF 15

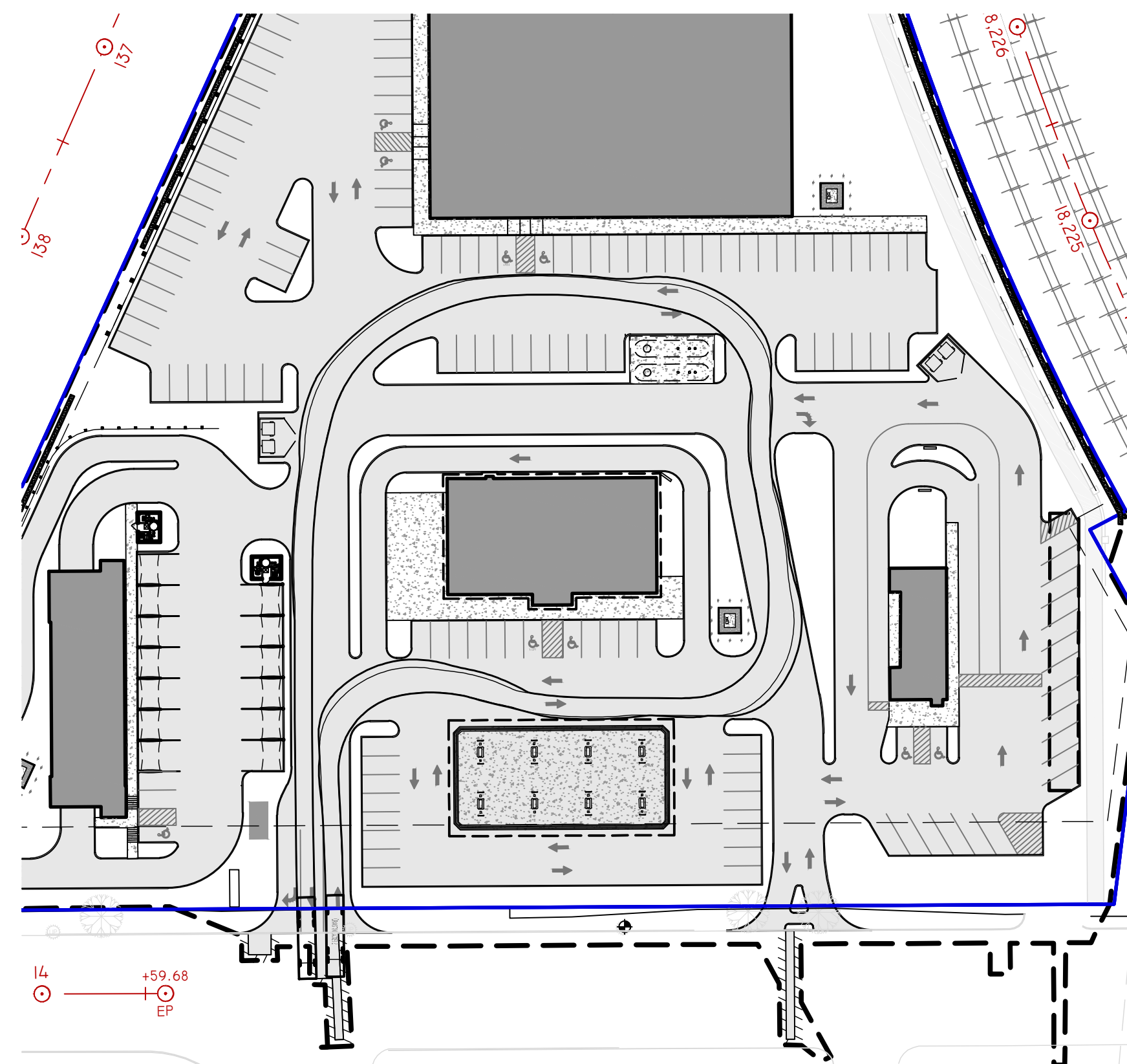




BUILDING #4  
TRUCK CIRCULATION

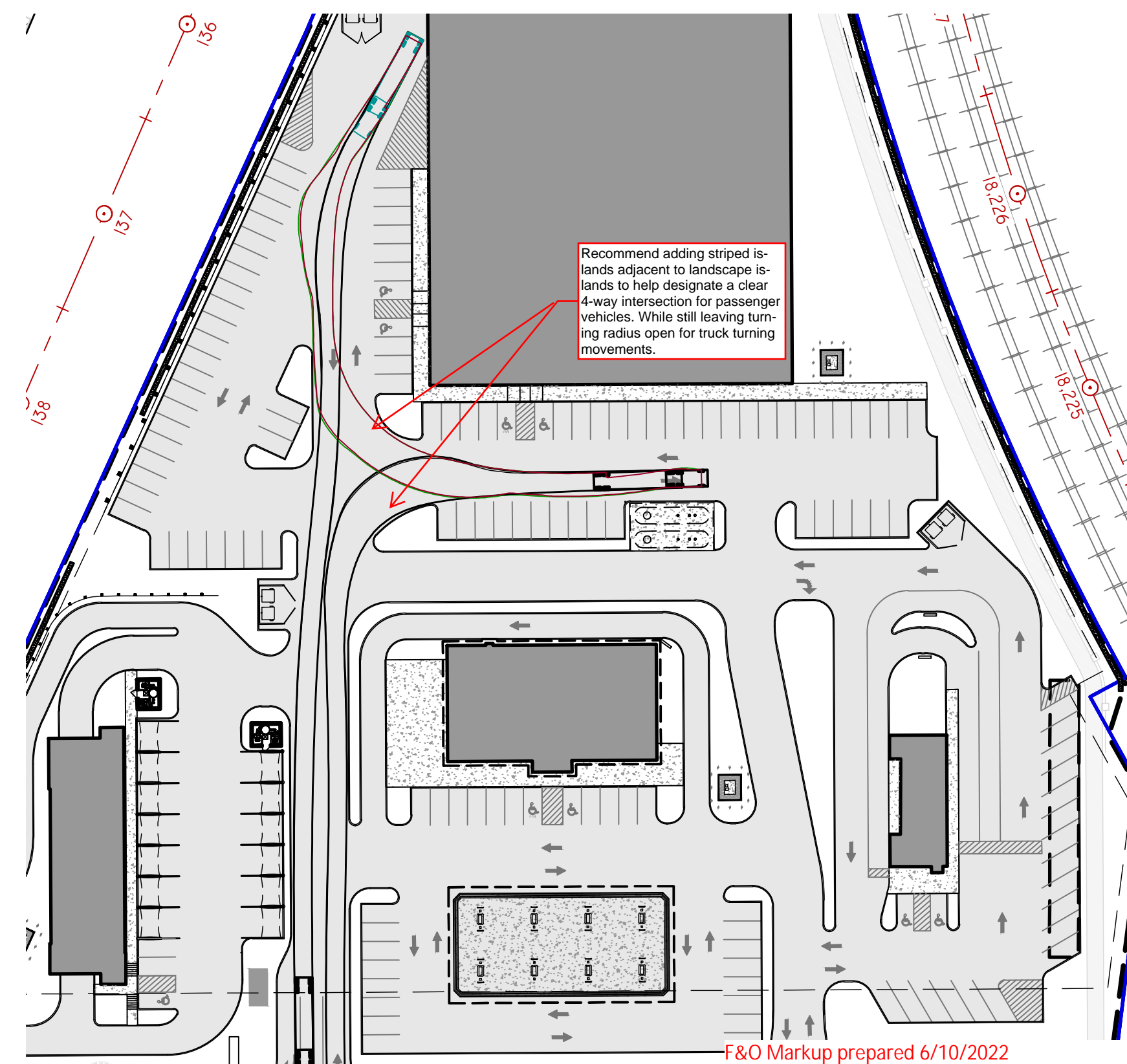


BUILDING #3  
FUEL TRUCK CIRCULATION



### BUILDING #3

### TRUCK CIRCULATION



BUILDING #1  
TRUCK CIRCULATION

F&O Markup prepared 6/10/2022  
Overall recommendations to improve site circulation  
Section 17.28.010 (B)(12) - Safe and Efficient Traffic  
Circulation

